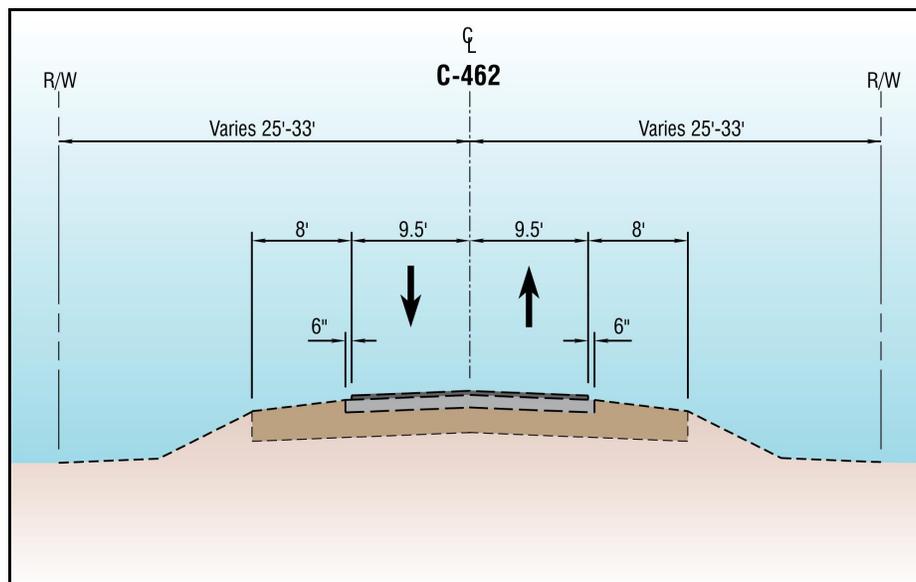


Study Overview

Kimley-Horn and Associates, Inc. was retained in April 2010 by Sumter County to perform a Preliminary Engineering Study (PES) for C-462, from CR 209 to C-466A. The purpose of the PES was to study the alignment of C-462 west of US 301 (C-462W) and C-462 east of US 301 (C-462E) and determine the future roadway geometry needs. C-462 is currently a two-lane undivided transitioning major county collector roadway within the study area, with very narrow lanes (9.5-feet) and no paved shoulders. The roadway is misaligned at the intersection of US 301, with C-462W connecting to US 301 approximately ¼ mile north of C-462E. The existing typical section is illustrated in *Figure 1* below.

Figure 1 – Existing Typical Section



The existing two-lane roadway section is not sufficient to meet traffic capacity and mobility needs within the 2035 planning horizon. Improvements to C-462 within the study area are anticipated to promote safety by bringing the roadway to current design standards, reduce operating costs by reducing congestion, improve traffic operations on US 301 by correcting the mis-alignment, and improve multi-modal travel through the inclusion of bicycle lanes and sidewalks

The PES developed design concepts based on engineering analysis and evaluated the social, economic, and environmental effects of the recommended alternative. Public input was solicited through public meetings, presentations, and stakeholder coordination meetings. *Table 1* summarizes the major project milestones.



Table 1: Project Milestones

Milestone	Date
Project Start Date	April 28 th , 2010
Public Alternatives Meeting	October 13 th , 2010
Draft PE Study Submittal	January 31 st , 2011
Lake-Sumter MPO TAC Presentation	February 9 th , 2011
Lake-Sumter MPO BPAC Presentation	February 10 th , 2011
Sumter County BOCC Workshop Presentation	March 15 th , 2011
Final PE Report Submittal	After Adoption

Corridor Alternatives

Two alternatives were evaluated; a no-build condition and a build condition. The build alternative typical section proposes to improve the current two-lane roadway section to have 12-foot travel lanes, left turn lanes, bike lanes, and sidewalks. West of US 301, left turn lanes will be provided at future development driveways. The roadway alignment west of US 301 will be modified to connect to C-462 east of US 301 at the existing signalized intersection. The O'Dell PUD will provide a total of 100-foot right-of-way through their site between CR 209 and US 301 for the re-alignment of C-462W with C-462E, per their development agreement with the City of Wildwood. The recommended alternative roadway section will be contained within the provided right-of-way.

East of US 301, the roadway section will have a continuous two-way left turn lane in areas where existing driveways are present, with raised medians placed in areas without existing or anticipated driveways. The roadway will be an urban section with closed drainage. The proposed typical section will be accommodated within the existing 66-foot roadway right-of-way. The only right-of-way that will need to be acquired for the build alternative is for a westbound right turn lane at the intersection of US 301 and for drainage pond locations.

The build-alternative typical sections are illustrated in *Figure 2* and *Figure 3*.



Kimley-Horn
and Associates, Inc.

C-462 Preliminary Engineering Study From C-475 to US 301

The build alternative was presented at the Public Alternatives meeting in October 2010. The public input was generally positive, with two individual property owners expressing concern about the re-alignment of C-462W through the O'Dell PUD. Following the Public Alternatives meeting and subsequent conversations with property owners, the connection of the old alignment of C-462W and the new alignment of C-462 was modified. Public input was also solicited regarding the re-design of the intersection of C-462 & CR 121. Three options were presented at the Public Alternatives meeting. There was no clear preference by the public of the presented options for the 462/121 intersection.

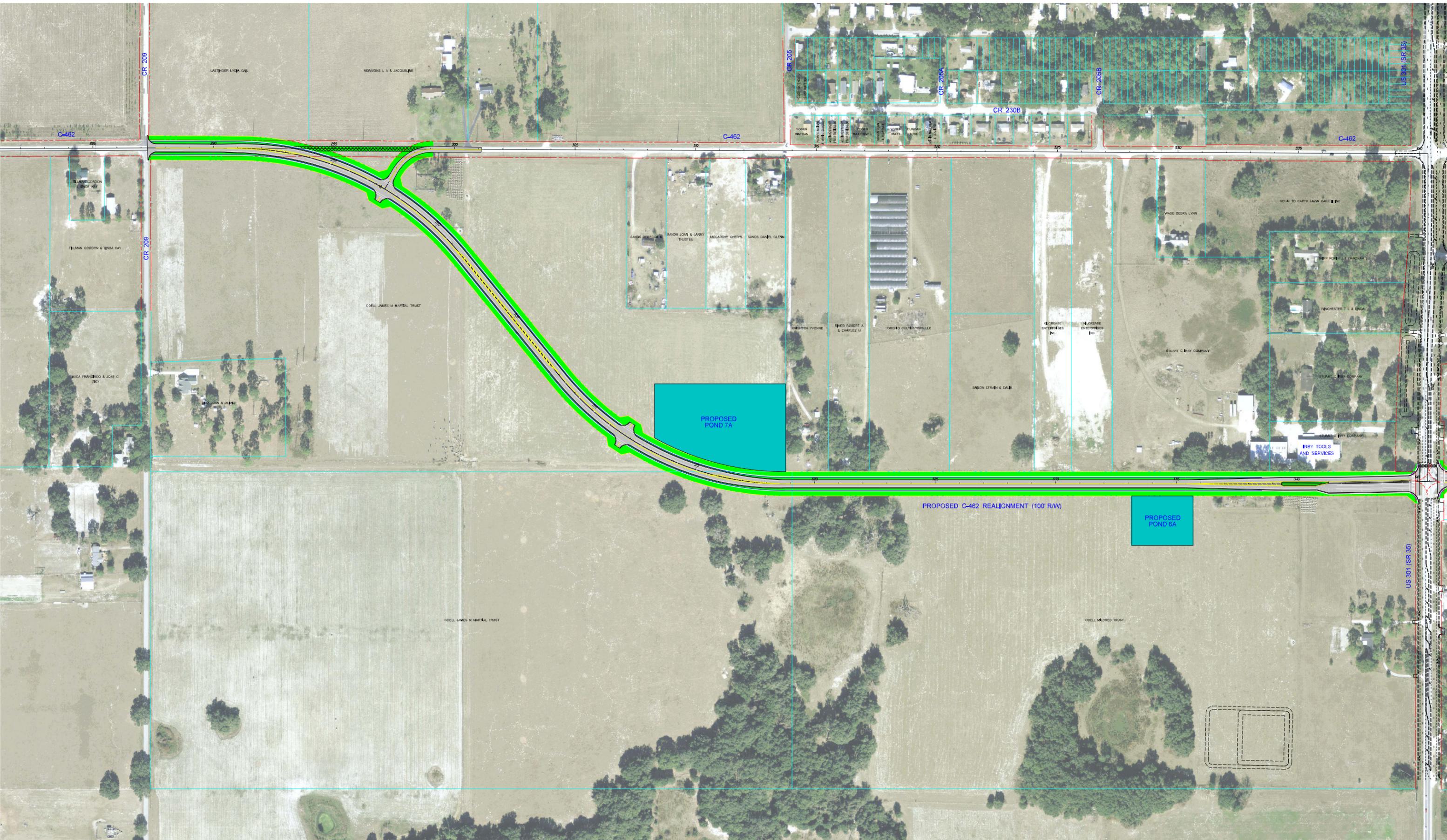
Preferred Alternative

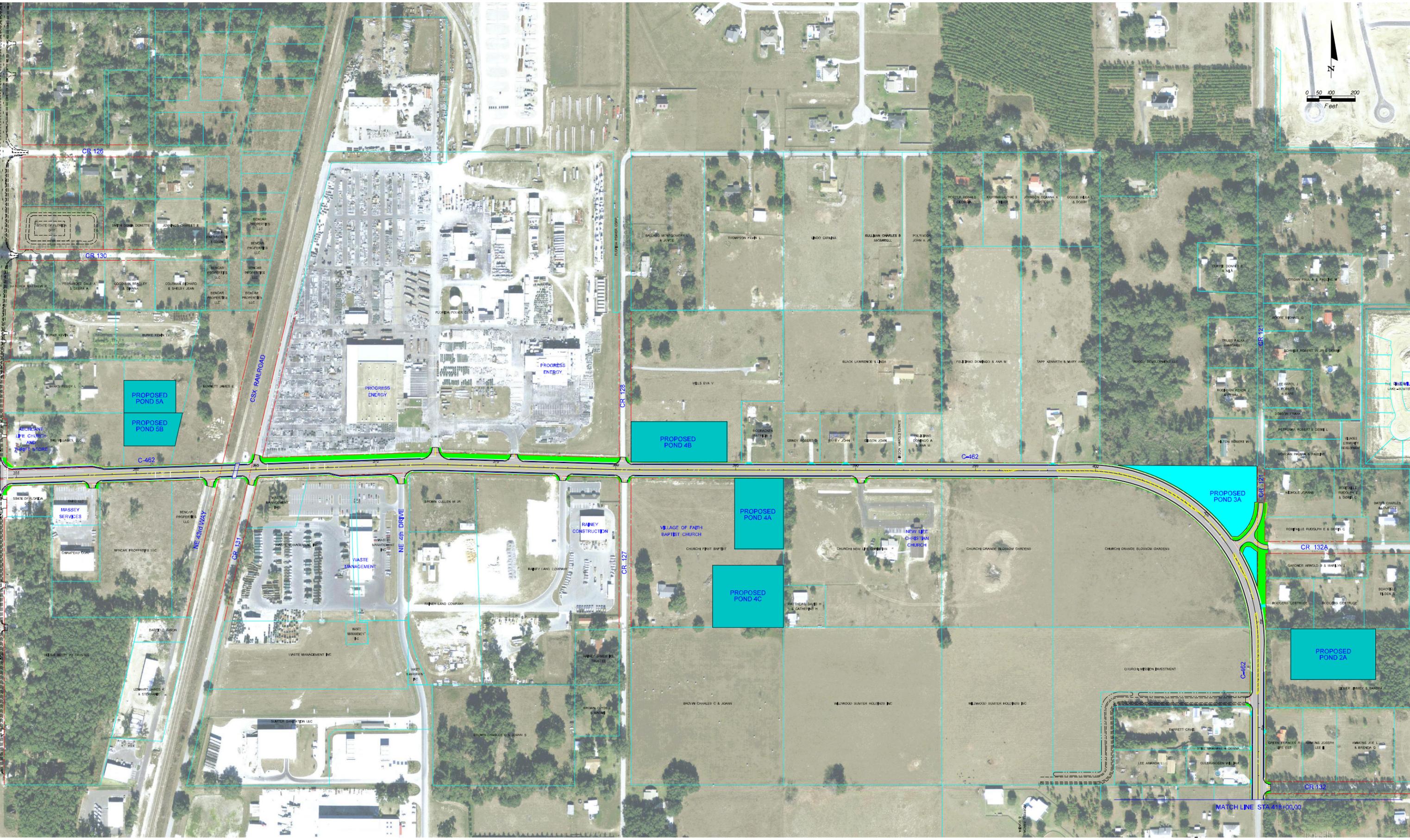
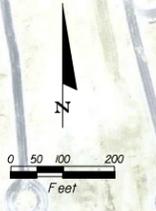
Through the public process, the build alternative was chosen as the Preferred Alternative. A preliminary engineering study was prepared documenting the design and decision process. The Preferred Alternative was presented at the Sumter County Board of County Commissioners Workshop on March 15th, 2011 where they agreed for Kimley-Horn to finalize the study with the Preferred Alternative. In summary, the Preferred Alternative:

- Improves the existing roadway by bringing it to current design standards,
- Provides a continuous connection between the segments east and west of US 301,
- Benefits users and improves safety by increasing capacity and reducing congestion,
- Improves traffic operations and safety at the intersection of C-462 & CR 121,
- Provides bicycle lanes and sidewalks,
- Requires approximately ¼ acre of right-of-way for the exclusive westbound left turn lane at the intersection of US 301 & C-462,
- Requires approximately 12 acres of right-of-way for drainage ponds,
- Involves 0.39 acres of wetland impacts,
- Estimated \$11.25 Million cost for design, right-of-way acquisition, and construction-related cost.

The estimated budgetary cost for the segment east of US 301 is \$6.8 Million.

The exhibits on the following pages illustrate the Preferred Alternative. The complete Preliminary Engineering Report may be viewed at the Public Works office.





PROPOSED POND 5A
PROPOSED POND 5B

PROPOSED POND 4B

PROPOSED POND 4A

PROPOSED POND 4C

PROPOSED POND 3A

PROPOSED POND 2A

MATCH LINE STA 418+00.00



MATCH LINE STA 4+19+00.00

CR 134C

CR 134B

ST. VINCENT DEPAUL CATHOLIC CHURCH

THE VILLAGES

THE VILLAGES OPERATING COMPANY

C-462

PROPOSED POND 1A

PROPOSED POND 1B

C-466A

CR 139 (POWELL RD)

