

Arnold, Bradley

From: Cottrell, Scott
Sent: Monday, July 11, 2011 12:56 PM
To: Arnold, Bradley
Cc: Jackson, Jackey; Wert, Chris
Subject: SRTS = Safe Routes to Schools - South Sumter Middle School
Attachments: South Sumter Middle Safe Routes to School Report - DRAFT 2011-07-01.pdf

Bradley,

See attached. Apparently Mike Woods thinks that Webster may not qualify for LAP certification to build these sidewalks and wants to know if we would take it on.

I have asked Mike 2 questions and have not heard back yet:

1. Why did Webster request this grant if they thought/knew they could not execute?
2. Could FDOT do it themselves?

As a back-up plan, PWD can do these if you so direct with CCNA consultant support and in-house Road and Bridge, but it just adds to our plate.

Scott
Scott B. Cottrell, PE
Public Works Director
319 E. Anderson Ave.
Bushnell, FL 33513
352-569-6700

From: Woods, Michael [mailto:mwoods@lakesumtermpo.com]
Sent: Wednesday, July 06, 2011 1:51 PM
To: Cottrell, Scott
Subject: South Sumter Middle School

Attached is the draft summary report for the South Sumter Middle School, FDOT Safe Routes to School Infrastructure Grant that the City of Webster submitted. It looks like it will be funded, in FY 12/13. **FDOT is currently looking for comments until July 15th.**

The City of Webster will have a hard time meeting the LAP certification requirements that are required to accept the FDOT funds. Would Sumter County consider being the LAP agent for this project?

Let me know, THX

Michael Woods
Transportation Planner
Lake~Sumter MPO
352-315-0170
www.LakeSumterMPO.com

SAFE ROUTE TO SCHOOL CONCEPT REPORT

NW 10th Avenue, NW 7th Street, and NW 6th Avenue
from South Sumter Middle School to NW 3rd Street
City of Webster
Sumter County, Florida

DRAFT

Prepared for:



Florida Department of Transportation
District 5 – Traffic Operations
DeLand, Florida

Prepared by:



Comprehensive Engineering Services, Inc.
201 South Orange Avenue, Suite 1300
Orlando, Florida 32801

July 2011

CES #06013.16-A

PROFESSIONAL ENGINEER CERTIFICATION

I hereby certify that I am a registered professional engineer in the State of Florida practicing engineering with **Comprehensive Engineering Services, Inc. (CES)**, and that I have supervised the preparation of and approve the analysis, findings, opinions, and technical advice hereby reported for:

PROJECT: NW 10th Avenue, NW 7th Street and NW 6th Avenue from
South Sumter Middle School to NW 3rd Street
Safe Route to School Concept Report
City of Webster, Sumter County, FL

The engineering work represented by this document was performed through the following duly authorized engineering business:

Comprehensive Engineering Services, Inc.

201 South Orange Avenue, Suite 1300

Orlando, Florida 32801-3417

Telephone: (407) 423-1600

Engineering Business Number (Board of Professional Engineers): **7862**

Expires 2/28/13

This report includes a brief discussion and evaluation of existing conditions and a constructability evaluation of a sidewalk path along the subject corridor. The conceptual design of the path was based upon applicable design criteria for the project and coordination with Sumter County and the Department. A cost estimate was also prepared for the construction of the path. I acknowledge that the procedures and references used to develop the conceptual design contained in this document are standard to the professional practice of transportation and traffic engineering as applied through professional judgment and experience.

Any engineering analysis, documents, conclusions or recommendations relied upon from other professional sources or provided with responsibility by the client are referenced accordingly in the following report.

FLORIDA PROFESSIONAL ENGINEER:

Ryan K. Baderschneider, P.E.

Name

REGISTRATION NUMBER: FL #60851.

SIGNATURE: _____.

DATE: _____.

1. PURPOSE & SCOPE:

The purpose of this analysis was to review and document the existing geometrics along NW 10th Avenue, NW 7th Street, and NW 6th Avenue in Webster, Florida to assess the construction effort required to incorporate a sidewalk along the corridor connecting South Sumter Middle School to NW 3rd Street. The City of Webster requested installation of a sidewalk as part of the FDOT's Safe Routes to School Program to accommodate local students who attend South Sumter Middle School. Note that this concept report does not include an engineering study reduced speed school zones and school crossings per the MUTCD and FDOT requirements.

As NW 10th Avenue, NW 7th Street, and NW 6th Avenue are local streets maintained by the City of Webster (not on the State Highway System), the primary design preferences considered in compiling this assessment were as follows:

FDOT's Manual on Uniform Minimum Standards for Design, Construction and Maintenance for Streets and Highways (May 2007)

Federal Highway Administration's Manual on Uniform Traffic Control Devices (2009)

2. EXISTING CONDITIONS:

As shown in the location map on the following page, the subject corridor is located in the northwest quadrant of the City of Webster, and west of SR 471. The project is contained within Section 36 of Township 21S Range 22E.

The posted regulatory speed limit along NW 6th Avenue and NW 10th Avenue is 25 MPH. There is no posted speed limit along NW 7th Street. School Zone speed limit signs are also posted in each direction along NW 10th Avenue approaching the South Sumter Middle School. A 20 MPH speed limit is applicable in front of the school when the flashing beacon on each sign is activated.

Based on the city map information obtained from the City of Webster and the parcel information obtained from the Sumter County Property Appraiser, it appears that the existing available right-of-way along NW 10th Avenue is 25 feet from the center of the roadway (50 feet total). Along NW 7th Street and NW 6th Avenue, it appears that the total existing available right-of-way is 25 feet wide. However, all of these roadways appear to meander as they have been recently paved to follow the alignment of the original dirt roadways. Therefore, the extent of the right-of-way could not be verified in the field and the following right-of-way assumptions have been made:

- The right-of-way along the south side of NW 10th Avenue is assumed to follow the fence line for South Sumter Middle School and is assumed to be roughly 25 feet from the center of the paved roadway between NW 7th Street to NW 3rd Street.

- The adjacent property to the west of NW 7th Street is owned by the school; therefore, available right-of-way has been assumed to extend to the fence line along west side of NW 7th Street. The right-of-way along the east side of NW 7th Street is assumed to follow the parcel lines from the property appraiser database.
- The right-of-way along the north side of NW 6th Avenue is assumed to follow the parcel lines from the property appraiser database. The right-of-way along the south side of NW 6th Avenue is assumed to be offset 25 feet from the assumed right-of-way on the north side.

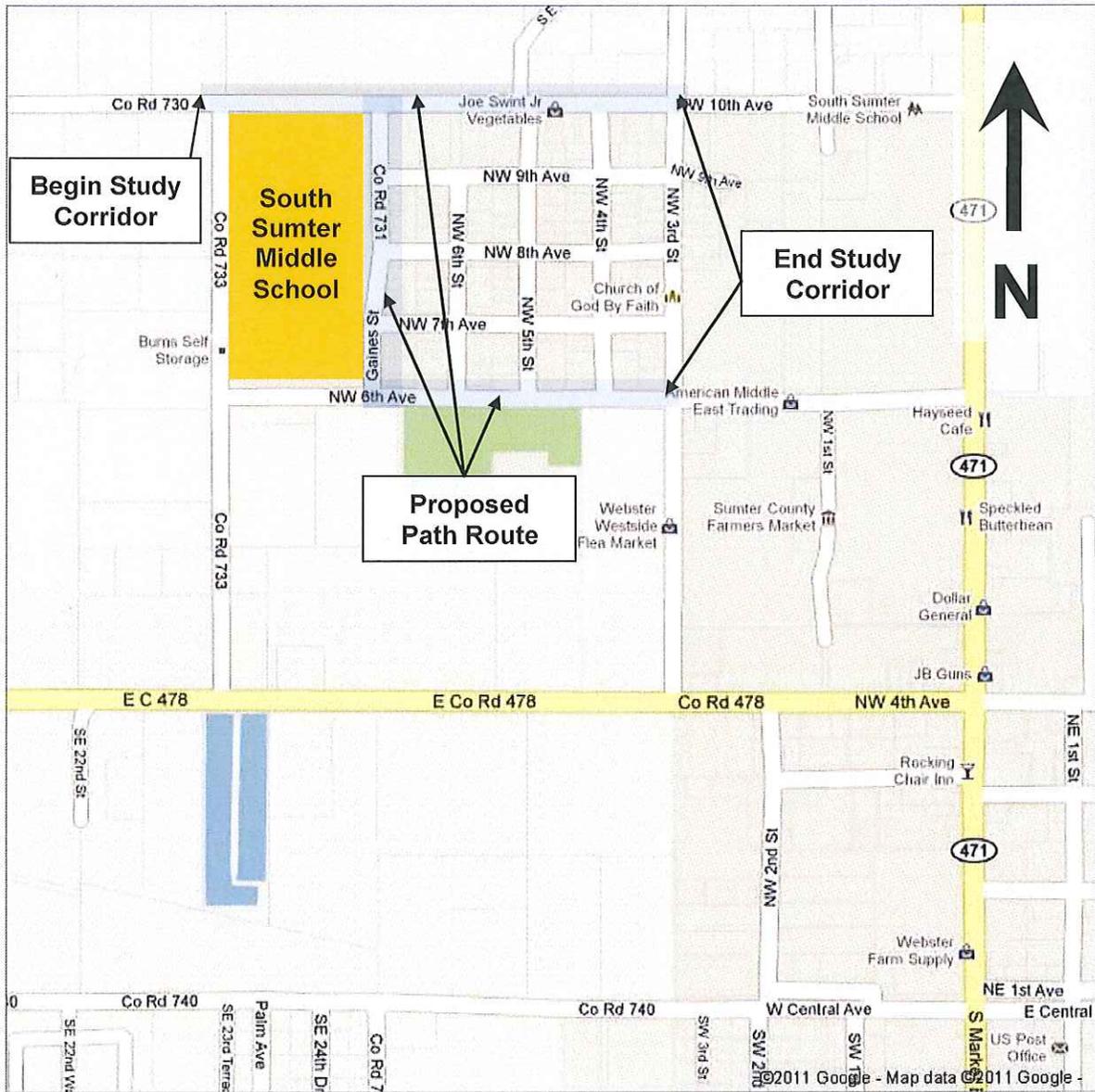
The typical section along NW 10th Avenue is a two-lane, undivided rural roadway with an open drainage system. Each direction of travel consists of a 9.5-10 foot wide travel lane with a 0-2 foot wide unpaved outside shoulder.

NW 7th Street is a two-lane, undivided rural roadway with an open drainage system. Each direction of travel consists of a 9 foot wide travel lane with a generally flat roadside to the right-of-way. There are no pavement markings along this roadway.

NW 6th Avenue is also a two-lane, undivided rural roadway with an open drainage system. Each direction of travel consists of an 8.5-9 foot wide travel lane with a generally flat roadside to the right-of-way which includes minor drainage depressions. There are no pavement markings along this roadway.

Photographs of the entire report corridor are included as Figure 2 following the location map.

FIGURE 1: PROJECT LOCATION MAP





Looking East Along NW 10th Ave. West of CR 733



Looking East Along South Side of NW 10th Ave. East of CR 733

**GENERAL PROJECT
PHOTOGRAPHS**



**FIGURE
2A**



Looking West South Along South Side of NW 10th Ave. at Drainage Swale Fronting School



Looking East Along South Side of NW 10th Ave. at Stormwater Retention Area Fronting School

**GENERAL PROJECT
PHOTOGRAPHS**



**FIGURE
2B**



Looking East Along South Side of NW 10th Ave. at NW 7th Street



Looking West Along South Side of NW 10th Ave. West of NW 5th Street

**GENERAL PROJECT
PHOTOGRAPHS**



**FIGURE
2C**



Looking East Along South Side of NW 10th Ave. East of NW 5th Street



Looking East Along South Side of NW 10th Ave. at NW 4th Street

**GENERAL PROJECT
PHOTOGRAPHS**



**FIGURE
2D**



Looking West Along South Side of NW 10th Ave. West of NW 3rd Street



Looking South Along West Side of NW 7th St. South of NW 10th Ave.

**GENERAL PROJECT
PHOTOGRAPHS**



**FIGURE
2E**



Looking South Along West Side of NW 7th St. South of Bus Loop Just North of 8th St.



Looking North Along West Side of NW 7th St. North of NW 6th Ave.

**GENERAL PROJECT
PHOTOGRAPHS**



**FIGURE
2F**



Looking East Along NW 6th Ave. east of NW 7th Street



Looking West Along NW 6th Ave. Approaching NW 7th Street

**GENERAL PROJECT
PHOTOGRAPHS**



**FIGURE
2G**



Looking East Along NW 6th Ave. East of NW 6th St.



Looking East Along South Side of NW 6th Ave. Approaching NW 5th Street

**GENERAL PROJECT
PHOTOGRAPHS**



**FIGURE
2H**



Looking East Along South Side of NW 6th Ave. Approaching NW 4th Street



Looking East Along South Side of NW 6th Ave. Approaching NW 3rd Street

**GENERAL PROJECT
PHOTOGRAPHS**



**FIGURE
21**

3. CONCEPTUAL DESIGN:

The basic criteria used for the conceptual design of sidewalk along the subject corridor were derived from the May 2007 Edition of FDOT's Manual on Uniform Minimum Standards for Design, Construction and Maintenance for Streets and Highways (Florida Greenbook) in addition to FDOT preferences for pavement markings, signing improvements and turnout construction. The 2009 Edition of the FHWA Manual on Uniform Traffic Control Devices (MUTCD) was also considered in determining the appropriate supplemental signing and pavement marking improvements related to the sidewalk.

The estimated right-of-way limits were derived from a review of right-of-way information available from the City of Webster and the Sumter County Property Appraiser, supplemented with field observations. Existing right-of-way maintenance maps are not available. See Section 2 for the assumptions used to estimate the available right-of-way along NW 10th Avenue, NW 7th Street, and NW 6th Avenue. The right-of-way limit designations shown on the conceptual diagrams should not be relied upon for construction purposes as the right-of-way limits were estimated and were based upon physical features discovered in the field (fences, utilities, etc.) and parcel data from the property appraiser database. Survey and right-of-way mapping should be completed prior to final design and will be required prior to construction.

Per the Safe Routes to School Infrastructure Application provided to the Department, it was desired to have a 5 foot wide sidewalk begin at South Sumter Middle School at the intersection of CR 733 and NW 10th Avenue, and continue east along the south side of NW 10th Avenue to the intersection at NW 3rd Street. A segment of sidewalk was also requested along the west side of NW 7th Street from NW 10th Avenue to NW 6th Avenue and along the south side of NW 6th Avenue from NW 7th Street intersection to NW 3rd Street. The application included several pedestrian school crossings and a proposed flashing school zone sign at the intersection of NW 7th Street and NW 6th Avenue (see "City of Webster SRTS Proposal Proposed Conditions" in Appendix).

After performing a field review and coordinating with the Department, modifications to the proposed design were developed. The modifications included the removal of the proposed flashing school zone sign along NW 6th Avenue and the addition of a landing area for the bus stop at the intersection of NW 10th Avenue and NW 6th Street.

A series of typical sections detailing the existing and proposed conditions is included at the end of this section as Figure 3. Seven typical sections are included to detail sidewalk construction for the three primary sidewalk design locations:

1. NW 10th Avenue from CR 733 to NW 3rd Street
2. NW 7th Street from NW 10th Avenue to NW 6th Avenue
3. NW 6th Avenue from NW 7th Street to NW 3rd Street

The conceptual sidewalk design in plan view format is included in the Appendix. The conceptual plan was developed solely for construction cost estimating (quantity) purposes only and is not intended for construction as survey data will be required to verify vertical

conditions and right-of-way. General anticipated design considerations for each of the three segments are as follows:

NW 10th Avenue from CR 733 to NW 3rd Street

- Construction of a five foot wide sidewalk is assumed to commence in the southwest corner of NW 10th Avenue and CR 733 and extend east along the south side of NW 10th Avenue toward NW 3rd Street. School crosswalks will be added across the side streets and school driveways.
- The roadside is flat from the edge of pavement to the existing fence adjacent to the school between CR 733 to approximately 60 feet east of the first driveway for the school on NW 10th Avenue. Sidewalk construction is anticipated to abut the fence line in this area (See Figure 3A).
- There is an existing drainage swale abutting the fence line from approximately 60 feet east of the first driveway for the school to the second driveway for the school on NW 10th Avenue. For this segment (as shown in Figure 3B), it is anticipated that the sidewalk will be placed one foot off the edge of pavement and a gravity wall, approximately 85 feet long, with handrail will be used in this area to minimize impacts to the drainage swale. Replacement of a mitered end section and extension of the storm drain pipe is also anticipated. During design, consideration may be given to filling the drainage swale and connecting the storm drain pipe, but will require formal drainage analysis.
- The roadside is flat from the edge of pavement to the fence line for the school between the second driveway for the school to approximately 140 feet east of this drive. At that point, there is a stormwater retention area which continues to NW 7th Street. This retention area has a front slope beginning approximately two feet from the edge of pavement. It is anticipated that the sidewalk will be placed two feet off the edge of pavement in this area and earthwork and sodding will be performed in the retention area to offset any volume impacts caused by the sidewalk construction. This typical section is detailed in Figure 3C.
- The roadside slopes away from the roadway between NW 7th Street and NW 5th Street. It is anticipated that the sidewalk will be placed three feet off of the edge of pavement in this area with embankment and sodding utilized for tying into the existing roadside grade. This typical section is also reflected in Figure 3C.
- The roadside is relatively flat from the edge of pavement to the right-of-way between NW 5th Street and NW 3rd Street. There are several obstacles that will need to be considered, including the potential removal of trees, relocating an existing wire fence, and relocating a power pole that is being used for a service drop. The large oak trees between NW 5th and NW 4th Street will require evaluation by an arborist if they are to remain. It is anticipated that the sidewalk will be placed from one to three feet from the edge of pavement in this area jogging closer to the edge of pavement and narrowing to four feet in width where necessary to avoid obstacles. This typical section is reflected in Figure 3D.
- Due to the potential for vehicle over-running of the sidewalk, six inch sidewalk should be incorporated where the sidewalk abuts the roadway, school driveways, and in front of residential unpaved drives along this segment. At residential unpaved drives, the sidewalk should be widened to abut the roadway. A sidewalk pad for a bus stop

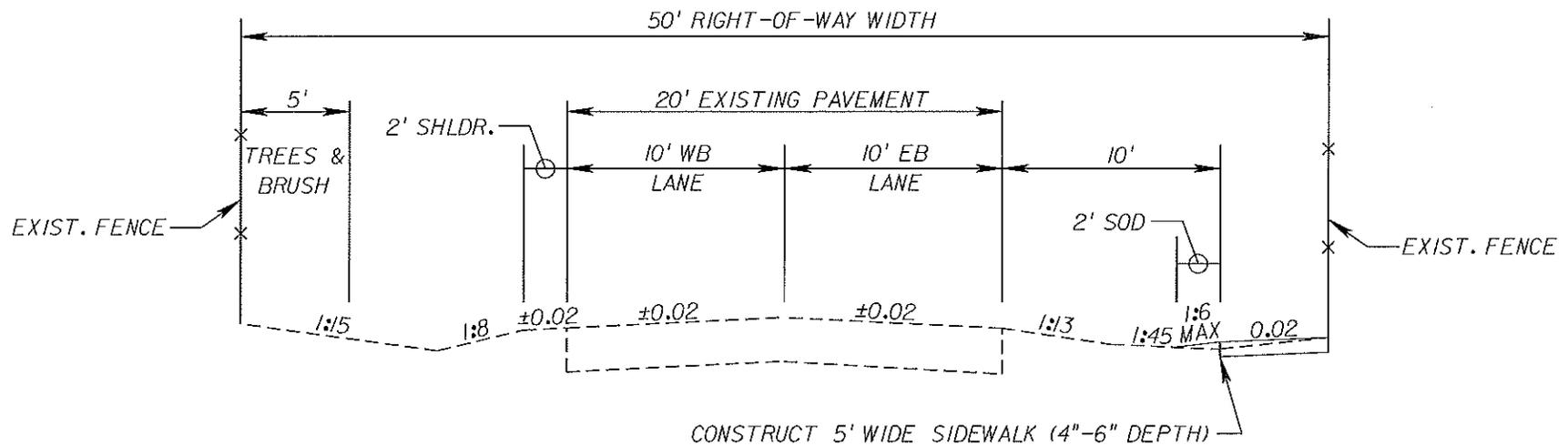
will also be constructed in the southwest corner of the NW 10th Avenue and NW 6th Street intersection.

NW 7th Street from NW 10th Avenue to NW 6th Avenue

- Construction of a five foot wide sidewalk is assumed to commence in the southwest corner of the intersection of NW 7th Street at NW 10th Avenue and extend south along the west side of NW 7th Street toward NW 6th Avenue. Per the application, school crosswalks will be added across NW 7th Street at NW 8th Avenue and NW 9th Avenue. A crosswalk will also be added across the driveway for the bus exit from the school.
- The roadside is generally flat from the edge of pavement to the fence line for the school. As shown in Figure 3E, it is anticipated that the sidewalk will be placed six feet from the edge of pavement in this area jogging further away from the edge of pavement to avoid utility obstacles as needed.
- Due to the potential for vehicle over-running of the sidewalk, six inch sidewalk should be incorporated where the sidewalk abuts the roadway and school driveways along this segment.

NW 6th Avenue from NW 7th Street to NW 3rd Street

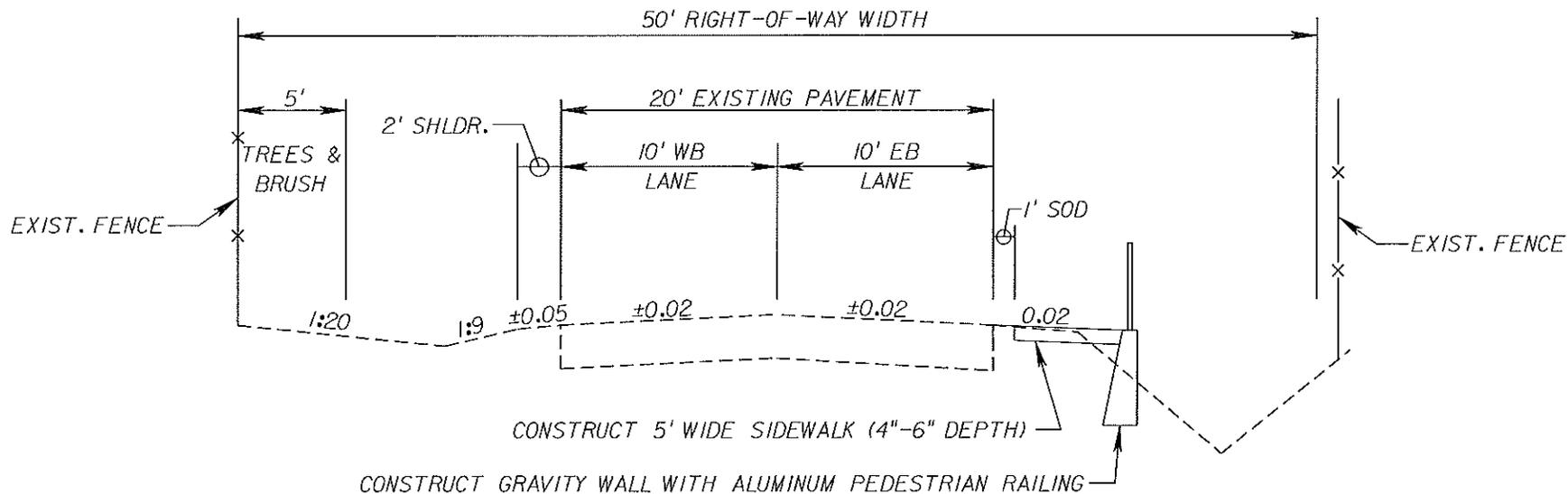
- Construction of a five foot wide sidewalk is assumed to commence in the southwest corner of NW 6th Avenue and NW 7th Street and extend east along the south side of NW 6th Avenue toward NW 3rd Street. A crosswalk will be added across NW 3rd Street.
- The roadside is relatively flat from the edge of pavement to the tree line between NW 7th Street and NW 6th Street. It is anticipated that the sidewalk will be placed three feet from the edge of pavement in this area as reflected in Figure 3F. The potential removal of a few large oak trees should be considered and will require evaluation by an arborist if they are to remain.
- Between NW 6th Street and NW 4th Street, it is anticipated that the sidewalk will be offset one foot from the existing fence line as shown in Figure 3G. The sidewalk is also anticipated to be narrowed at locations of existing power poles. An existing drainage depression lies between the roadway and the proposed sidewalk.
- The separation from the edge of roadway pavement to the right-of-way line narrows between NW 4th Street and NW 3rd Street. It is anticipated that the sidewalk width will be narrowed to three feet in the segment and a design variation will be obtained. The design must also incorporate a passing space at an interval not to exceed 200 feet. An existing drainage depression lies between the roadway and the proposed sidewalk.
- Due to the potential for vehicle over-running of the sidewalk, six inch sidewalk should be incorporated where the sidewalk abuts the roadway along this segment



* EXISTING CONDITION DERIVED FROM CROSS SECTION MEASURED ALONG THE CORRIDOR.
 LOCATION OF RIGHT-OF-WAY LIMITS SHOWN ARE APPROXIMATE. VERTICAL SCALE
 EXAGGERATED TO SHOW SLOPE BREAKS AND TOPOGRAPHY CHANGES.

PROPOSED TYPICAL SECTION (1 OF 7)
 (NW 10TH AVENUE - JUST EAST OF CR 733)

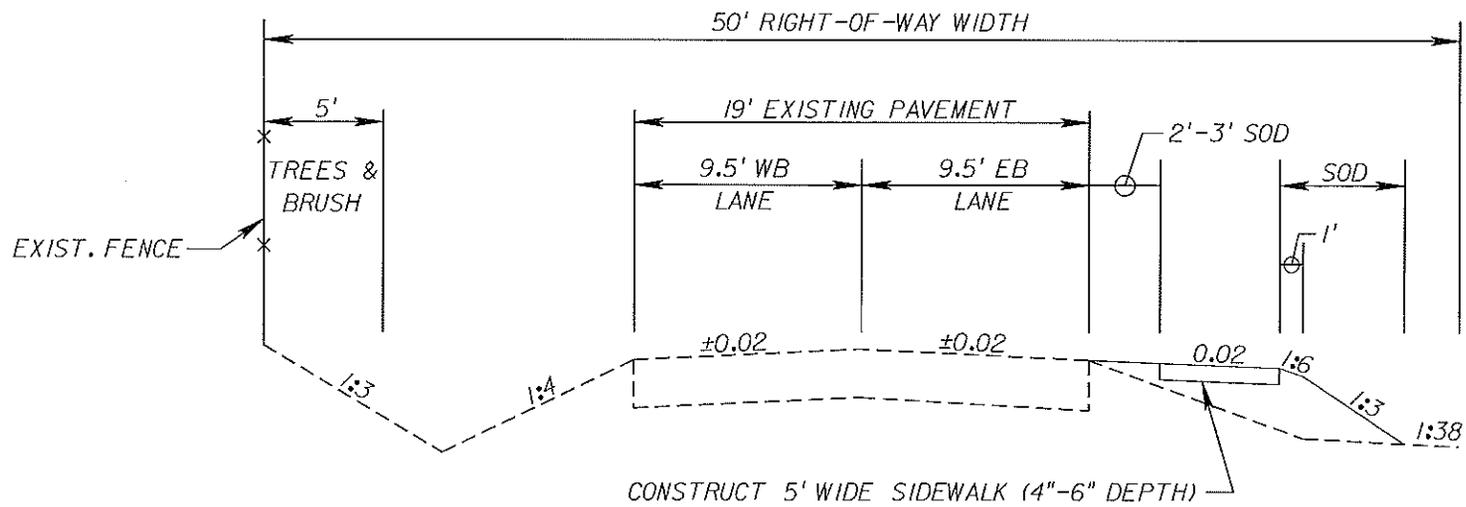
FIGURE 3A



* EXISTING CONDITION DERIVED FROM CROSS SECTION MEASURED ALONG THE CORRIDOR.
 LOCATION OF RIGHT-OF-WAY LIMITS SHOWN ARE APPROXIMATE. VERTICAL SCALE
 EXAGGERATED TO SHOW SLOPE BREAKS AND TOPOGRAPHY CHANGES.

PROPOSED TYPICAL SECTION (2 OF 7)
 (NW 10TH AVENUE - WEST OF NW 7TH STREET)

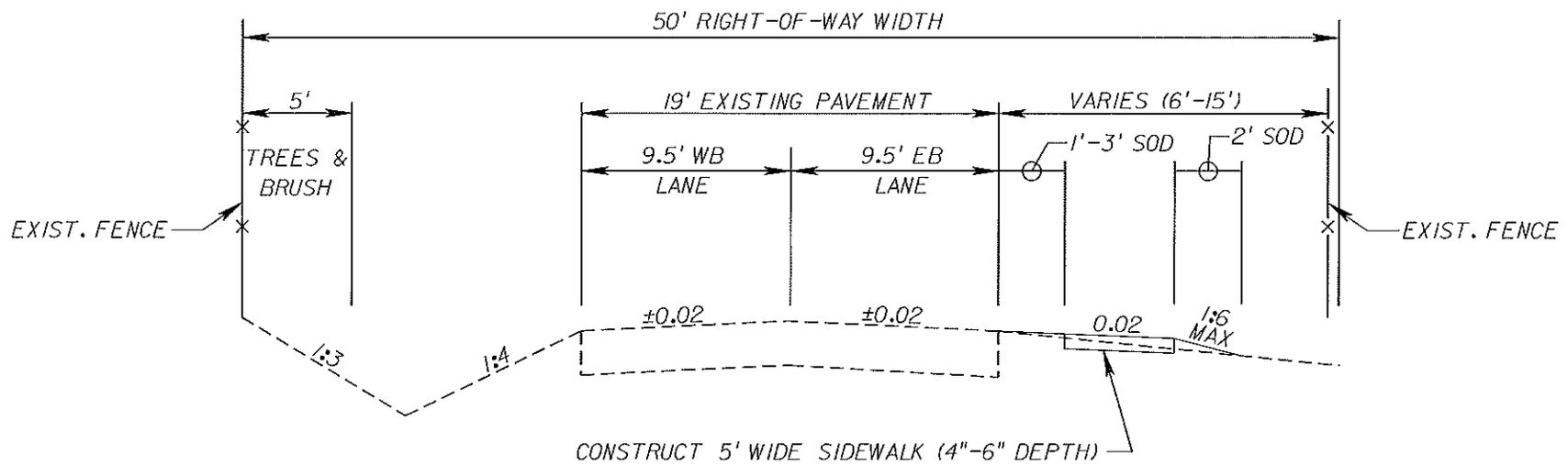
FIGURE 3B



* EXISTING CONDITION DERIVED FROM CROSS SECTION MEASURED ALONG THE CORRIDOR.
 LOCATION OF RIGHT-OF-WAY LIMITS SHOWN ARE APPROXIMATE. VERTICAL SCALE
 EXAGGERATED TO SHOW SLOPE BREAKS AND TOPOGRAPHY CHANGES.

PROPOSED TYPICAL SECTION (3 OF 7)
 (NW 10TH AVENUE - FROM WEST OF NW 7TH ST.
 TO NW 5TH ST.)

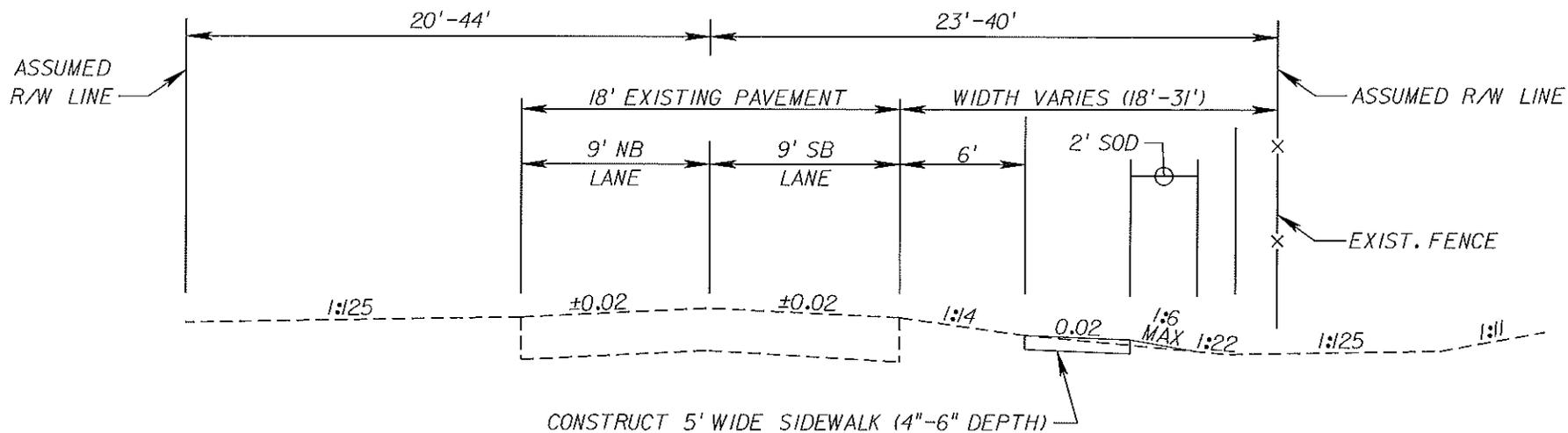
FIGURE 3C



* EXISTING CONDITION DERIVED FROM CROSS SECTION MEASURED ALONG THE CORRIDOR.
 LOCATION OF RIGHT-OF-WAY LIMITS SHOWN ARE APPROXIMATE. VERTICAL SCALE
 EXAGGERATED TO SHOW SLOPE BREAKS AND TOPOGRAPHY CHANGES.

PROPOSED TYPICAL SECTION (4 OF 7)
 (NW 10TH AVENUE - FROM NW 5TH ST.
 TO NW 3RD ST.)

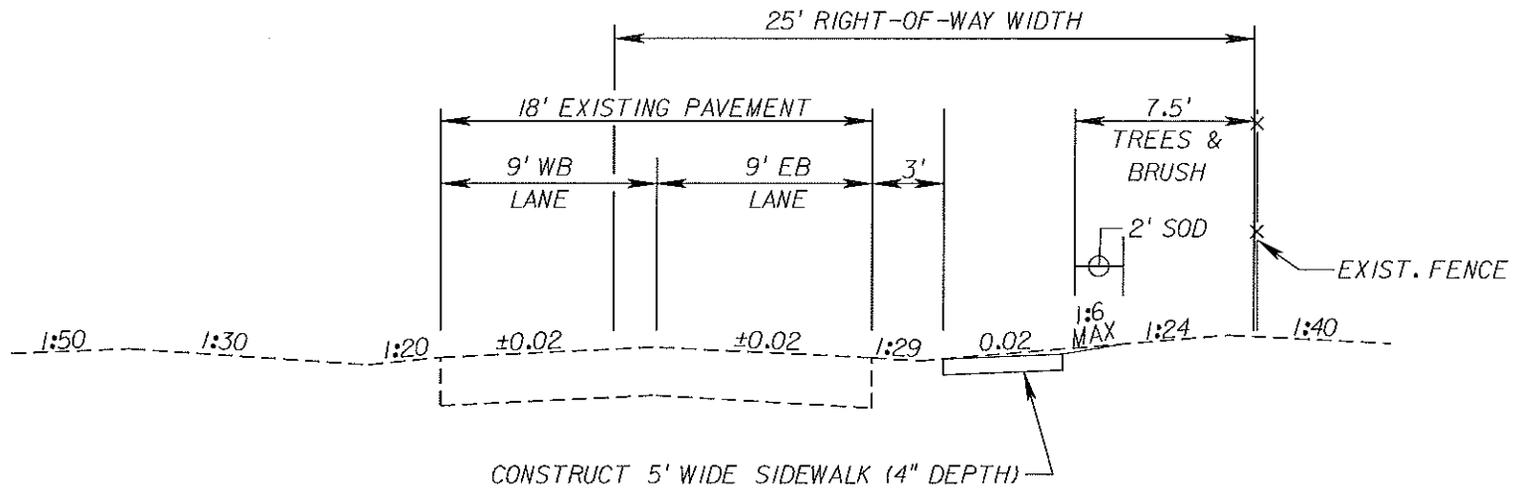
FIGURE 3D



* EXISTING CONDITION DERIVED FROM CROSS SECTION MEASURED ALONG THE CORRIDOR.
 LOCATION OF RIGHT-OF-WAY LIMITS SHOWN ARE APPROXIMATE. VERTICAL SCALE
 EXAGGERATED TO SHOW SLOPE BREAKS AND TOPOGRAPHY CHANGES.

PROPOSED TYPICAL SECTION (5 OF 7)
 (NW 7TH STREET - NW 10TH AVE.
 TO NW 6TH AVE.)

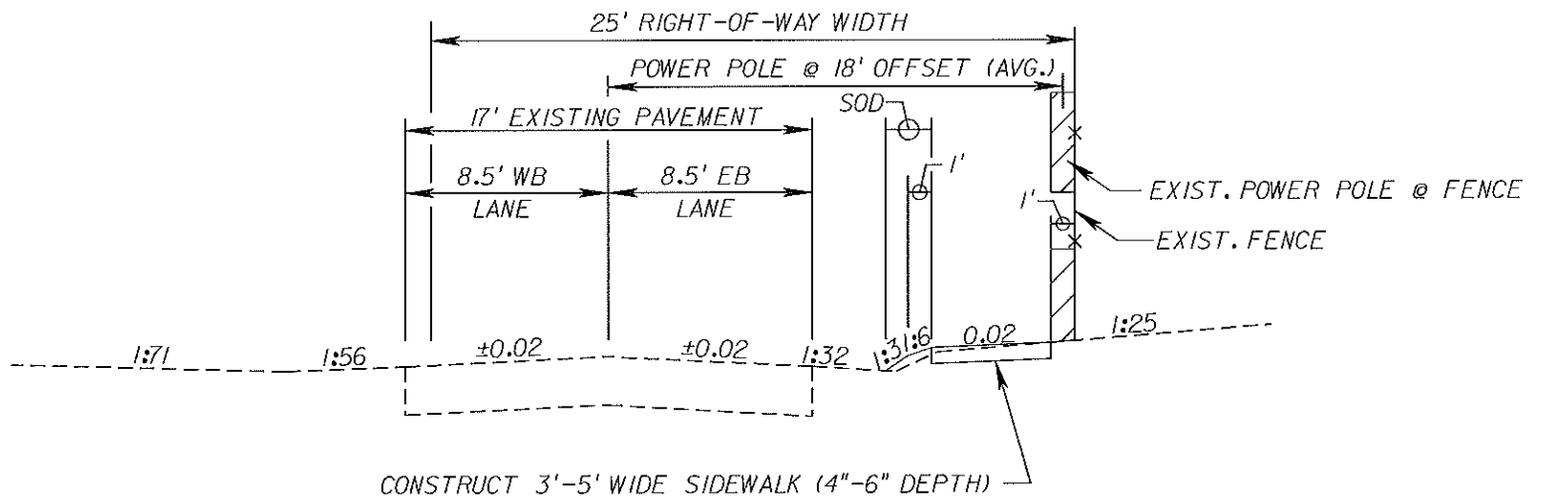
FIGURE 3E



* EXISTING CONDITION DERIVED FROM CROSS SECTION MEASURED ALONG THE CORRIDOR.
 LOCATION OF RIGHT-OF-WAY LIMITS SHOWN ARE APPROXIMATE. VERTICAL SCALE
 EXAGGERATED TO SHOW SLOPE BREAKS AND TOPOGRAPHY CHANGES.

PROPOSED TYPICAL SECTION (6 OF 7)
 (NW 6TH AVENUE - NW 7TH ST.
 TO NW 6TH ST.)

FIGURE 3F



* EXISTING CONDITION DERIVED FROM CROSS SECTION MEASURED ALONG THE CORRIDOR.
 LOCATION OF RIGHT-OF-WAY LIMITS SHOWN ARE APPROXIMATE. VERTICAL SCALE
 EXAGGERATED TO SHOW SLOPE BREAKS AND TOPOGRAPHY CHANGES.

PROPOSED TYPICAL SECTION (7 OF 7)
 (NW 6TH AVENUE - NW 6TH ST.
 TO NW 3RD ST.)

FIGURE 3G

4. CONSTRUCTION COST ESTIMATE:

The total cost for the project was estimated based on the quantified items obtained from the conceptual design. Additional items provided in the cost estimate include maintenance of traffic (MOT), engineering and inspection services (PE/CEI), mobilization and construction contingencies. The unit cost items included in the estimate were primarily obtained from the Department's recent 12-month unit cost history for Sumter County (June 2010 through May 2011). If unit cost items were not available from this source, statewide data was considered.

The total estimated cost of the project is detailed in Table 1 on the following page. Including design, maintenance of traffic and contingencies, the estimated cost for the project is \$268,504.38.

PRELIMINARY ENGINEER'S CONSTRUCTION COST ESTIMATE
FINANCIAL PROJECT ID
South Sumter Middle School - Safe Routes to School

6/29/2011

PAY ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	STATE AVERAGE UNIT COST *	
ROADWAY					
104-10-3	SEDIMENT BARRIER	LF	4403	\$0.74	\$3,258.22
110-1-1	CLEARING AND GRUBBING	AC	0.82	\$4,161.04	\$3,407.35
120-1	REGULAR EXCAVATION	CY	294	\$2.04	\$598.79
120-6	EMBANKMENT	CY	796	\$4.49	\$3,572.21
400-011	CLASS NS CONCRETE, GRAVITY WALL	CY	18	\$596.00	\$10,882.96
425-5	MANHOLE, ADJUST	EA	1	\$835.00	\$835.00
425-6	VALVE BOXES, ADJUST	EA	6	\$135.00	\$810.00
430-174-118	PIPE CULVERT OPTIONAL MATERIAL, ROUND 0-24" SD	LF	7	\$23.12	\$161.84
430-984-125	MITERED END SECTION (OPTIONAL ROUND 18" SD)	EA	1	\$610.54	\$610.54
515-1-2	PIPE HANDRAIL - GUIDERAIL, ALUMINUM	LF	83	\$22.00	\$1,826.00
522-1	CONCRETE SIDEWALK (4" THICK)	SY	2133	\$22.23	\$47,424.00
522-2	CONCRETE SIDEWALK (6" THICK)	SY	267	\$65.04	\$17,394.59
550-10-918	FENCING, SPECIAL TYP., 0'-5', RESET EXIST.	LF	101	\$50.00	\$5,050.00
570-1-2	PERFORMANCE TURF, SOD	SY	3149	\$1.51	\$4,754.82
ROADWAY TOTAL =					\$100,586.32
SIGNING AND PAVEMENT MARKING					
700-20-11	SINGLE POST SIGN, F&I, LESS THAN 12SF	AS	14	\$216.13	\$3,025.82
700-20-40	SINGLE POST SIGN, RELOCATE	AS	12	\$26.36	\$316.32
711-11-123	THERMOPLASTIC (WHITE SOLID 12")	LF	890	\$1.77	\$1,575.30
711-11-125	THERMOPLASTIC (WHITE SOLID 24")	LF	996	\$3.74	\$3,725.04
711-11-160	THERMOPLASTIC (WHITE MESSAGE)	EA	6	\$82.62	\$495.72
711-17	THERMOPLASTIC (REMOVE)	SF	26	\$3.00	\$78.00
SIGNING AND PAVEMENT MARKING TOTAL =					\$8,642.48
PROJECT SUBTOTAL					\$109,228.80
Mobilization (15%)					\$16,384.32
Maintenance of Traffic (20%)					\$21,845.76
Erosion Control (5%)					\$5,461.44
PROJECT TOTAL					\$152,920.32
Contingency (20%)					\$30,584.06
PROJECT GRAND TOTAL (CONSTRUCTION ONLY)					\$183,504.38
PE/CEI (Per Study)					\$85,000.00
PROJECT GRAND TOTAL (PE/CEI & CONSTRUCTION)					\$268,504.38

* FDOT Sumter County (Area 7) Average Unit Cost from 2010/6/01 to 2011/5/31

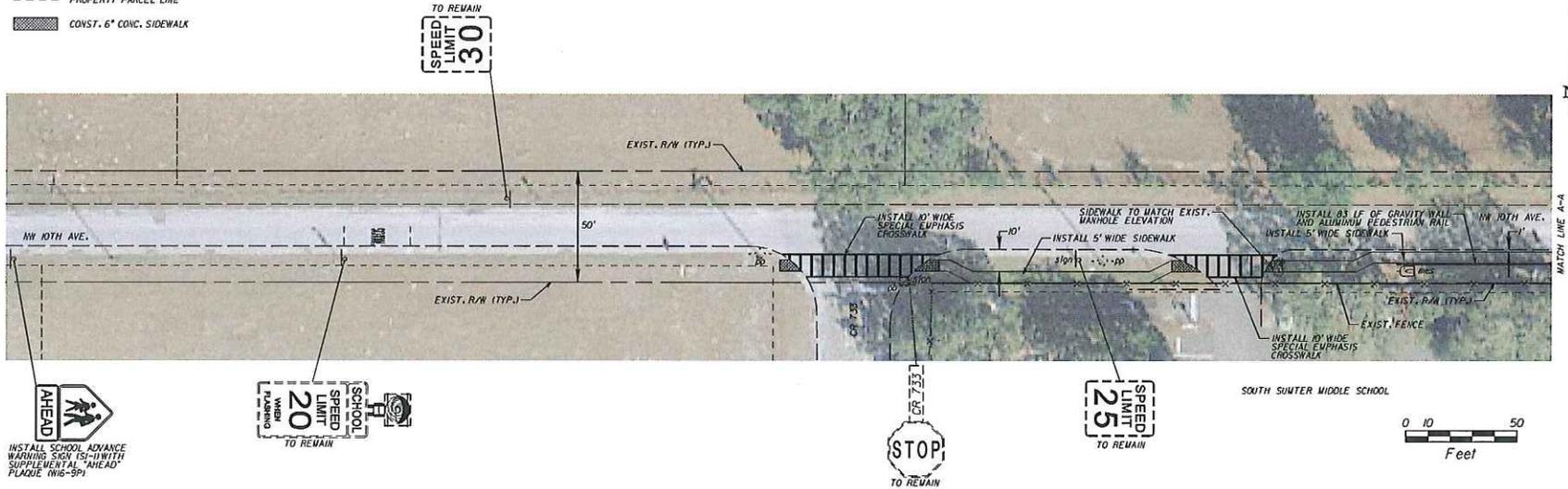
** Detectable Warning Surface Installations on proposed sidewalk sections are incidental to the cost of sidewalk construction

APPENDIX

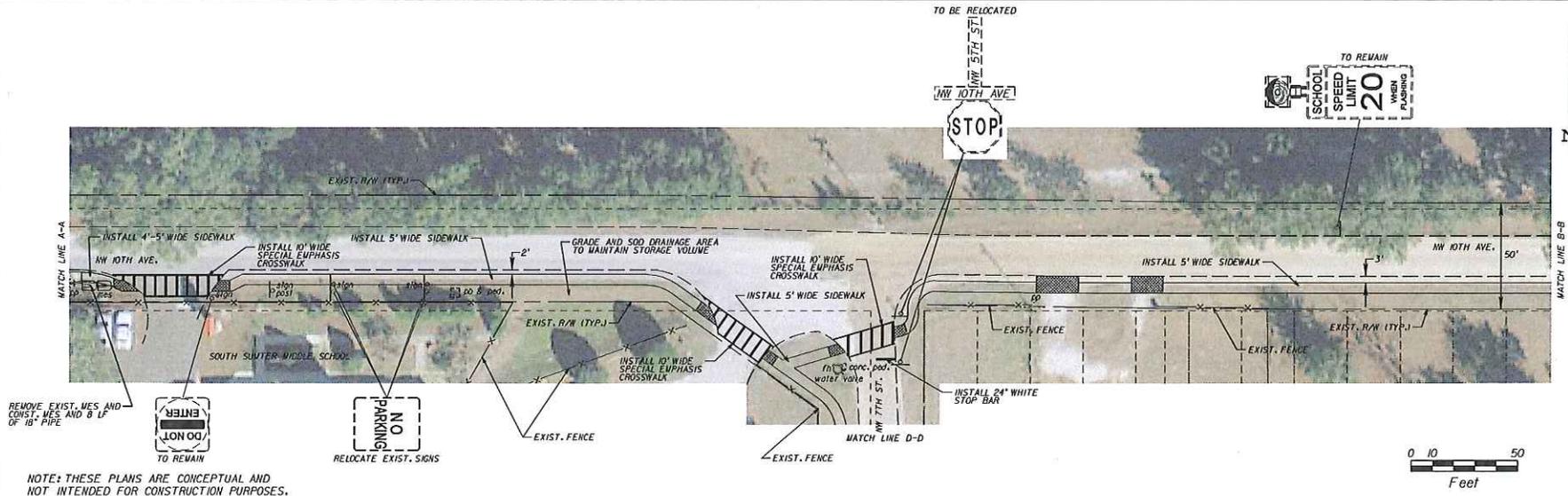
South Sumter Middle School Safe Routes to School Conceptual Sidewalk Plans

Right-of-Way Information

LEGEND:
 - - - - - PROPERTY PARCEL LINE
 [Hatched Box] CONST. 6" CONC. SIDEWALK



INSTALL SCHOOL ADVANCE WARNING SIGN FS-11 WITH SUPPLEMENTAL "AHEAD" PLAQUE (RW-5P)



REMOVE EXIST. MES AND CONST. MES AND 8 LF OF 18" PIPE

NOTE: THESE PLANS ARE CONCEPTUAL AND NOT INTENDED FOR CONSTRUCTION PURPOSES.

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

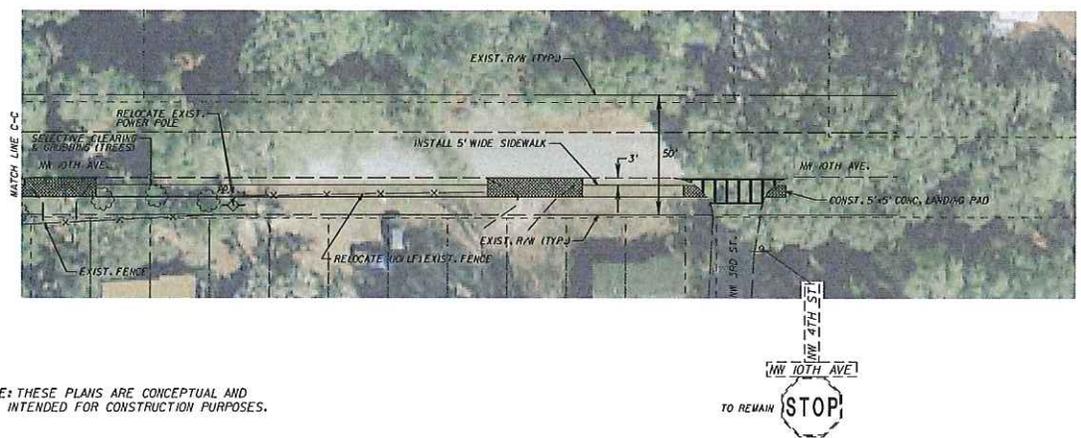
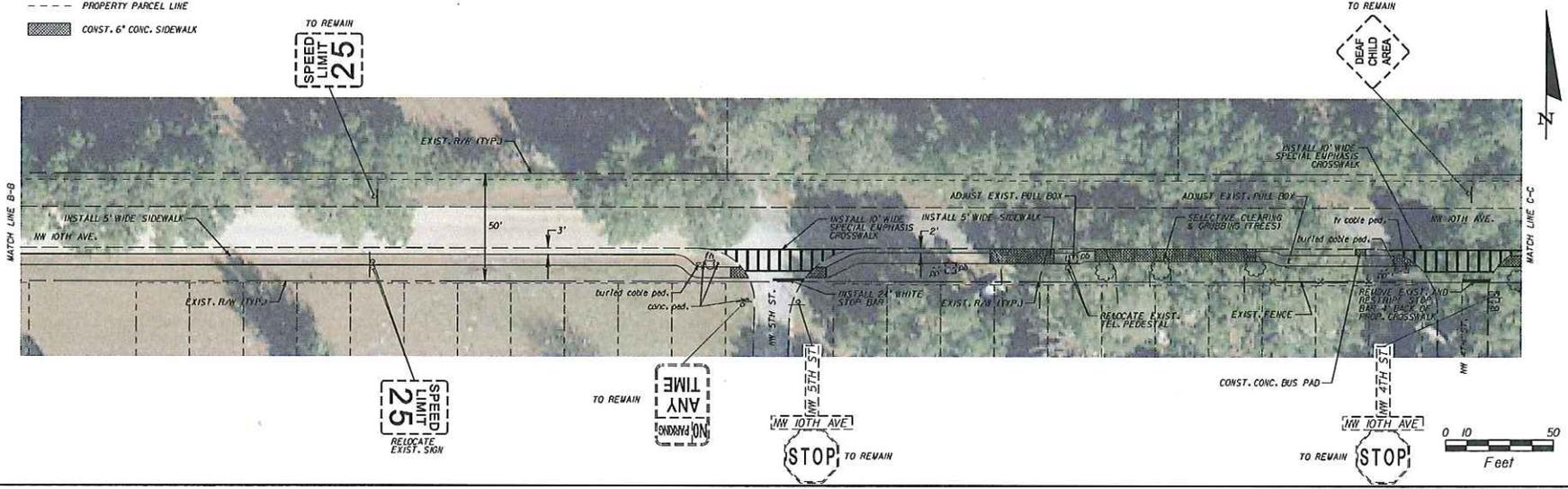


STATE OF FLORIDA
 DEPARTMENT OF TRANSPORTATION
 PROJECT DESCRIPTION
 SOUTH SUMTER MIDDLE
 SAFE ROUTES TO SCHOOL

CONCEPT DIAGRAM

SHEET NO.
 A-1

LEGEND:
 - - - - - PROPERTY PARCEL LINE
 [Hatched Box] CONST. 6' CONC. SIDEWALK



NOTE: INSTALL SCHOOL ADVANCE WARNING SIGN (S1-1) WITH SUPPLEMENTAL "AHEAD" PLAQUE (W16-9P) AND "SCHOOL" PAVEMENT MESSAGE 200' IN ADVANCE OF THE INTERSECTION OF NW 10TH AVE. AT NW 3RD ST., NW 4TH ST., AND NW 5TH ST.

NOTE: THESE PLANS ARE CONCEPTUAL AND NOT INTENDED FOR CONSTRUCTION PURPOSES.

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

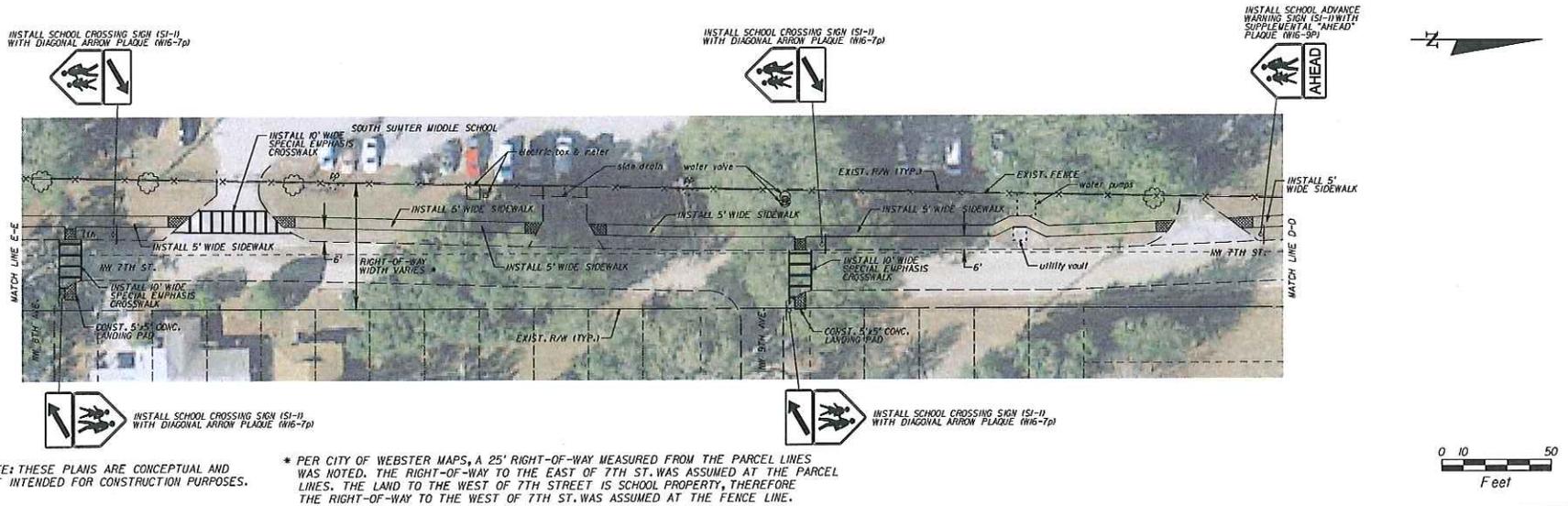
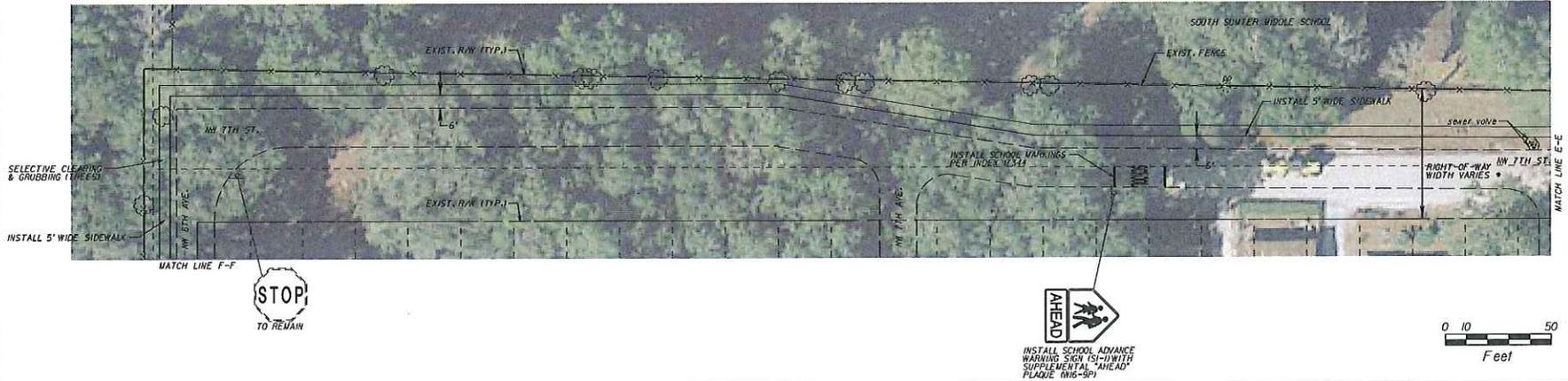


STATE OF FLORIDA
 DEPARTMENT OF TRANSPORTATION
 PROJECT DESCRIPTION
 SOUTH SUMNER MIDDLE
 SAFE ROUTES TO SCHOOL

CONCEPT DIAGRAM

SHEET NO.
 A-2

- LEGEND:**
 - - - - - PROPERTY PARCEL LINE
 [Hatched Box] CONST. 6" CONC. SIDEWALK



NOTE: THESE PLANS ARE CONCEPTUAL AND NOT INTENDED FOR CONSTRUCTION PURPOSES.

* PER CITY OF WEBSTER MAPS, A 25' RIGHT-OF-WAY MEASURED FROM THE PARCEL LINES WAS NOTED. THE RIGHT-OF-WAY TO THE EAST OF 7TH ST. WAS ASSUMED AT THE PARCEL LINES. THE LAND TO THE WEST OF 7TH STREET IS SCHOOL PROPERTY, THEREFORE THE RIGHT-OF-WAY TO THE WEST OF 7TH ST. WAS ASSUMED AT THE FENCE LINE.

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION



STATE OF FLORIDA
 DEPARTMENT OF TRANSPORTATION
 PROJECT DESCRIPTION
 SOUTH SUMNER MIDDLE
 SAFE ROUTES TO SCHOOL

CONCEPT DESIGN

SHEET NO.
 A-3

